

S. No.	Parameter	Guidelines	
7	Parking facilities	<p>Any parking facilities around the Heritage Zone need to be provided based on the standard ratio and tourist footfall, surrounding land-use and the availability of open private or public land. The parking facility should be between 100 m to 500 m distance from the Heritage Zone.</p> <p>For any building in the immediate vicinity of the Heritage Zone shall provide a basement parking (4 wheel) compulsory for its own use and visitors use. Tourist buses shall not be allowed near the Heritage Zone and shall strictly follow the traffic management and designated parking policy to be implemented within the designated HRIDAY or Heritage area.</p> <ul style="list-style-type: none"> - The DPR consultant need to design the parking spaces around the Heritage Zone based on the tourist footfall data and surrounding land-use and activity. - It shall also take into consideration the underground services and other road levels (also future development and resurfacing of municipal roads) while designing the parking facility. - The parking facility shall have clear designation for 2 wheeler, 4 wheelers, mini-buses and a VIP vehicles. The width of the pathway of the parking shall adhere to the minimum standards highlighted in the DCR and shall be designed considering the access and egress for emergency vehicles including medical vans, fire-fighting vehicles and police vehicles. - There needs to be separate entry and exit gates with watchman and security check cabins on both gates. - Service infrastructure provision for installing CCTV cameras systems, specified by the police department shall be compulsorily provided in the overall design of the parking facility. - The design of the gates shall ensure non-accessibility to stray cattle, shall ensure no person can climb the gates easily to enter the parking facility. - The design of parking facility shall have a defined pedestrian path directly leading to the Heritage Zone and shall be barrier free. - A service cabin which includes a toilet space, pantry area, storage area and emergency space for watchman needs to be provided within the parking facility and shall be well connected with the municipal sewer network. 	
8	Connectivity	<p>To Public transport / Intermediate Public transport</p> <ul style="list-style-type: none"> - There should be a pedestrian accessibility to the nearest bus stand or public transport facility from the Heritage Zone. ‘OR’ - There should be a pedestrian accessibility to the nearest designated IPT stand available or designed between 100 m and 500 m vicinity of the Heritage Zone. 	<p>To the Main temple</p> <ul style="list-style-type: none"> - There should be a clearly defined pedestrian path from the Heritage Zone connected to the primary road leading towards procession path or any path leading to the temple which is identified in the City HRIDAY Plan. - The path shall be accommodated within the available ROW and shall follow the required guidelines which are provided in the ‘Public Streetscape’ toolkit. <p>- Connectivity for senior citizens and people with disability is essential from the nearest designated or available parking facility to the Heritage Zone and there a special provision of public pathway for maneuvering of vehicles such as battery operated carts, tricycle, wheelchairs, etc. which are registered and passed by/with the local police department are necessary.</p>

1.1. Public Streetscape Guideline Toolkit

Public streetscape toolkit will enable the consultant to understand the gaps identified by the HCA and will provide a framework to prepare a comprehensive DPR highlighting the needs, to fill the gaps prevailing in the city with respect to the vision of the City HRIDAY Plan.

This toolkit shall be largely applicable throughout the HRIDAY delineated area and specifically applicable to streets leading to **Heritage zones and plazas** mentioned in the CHP document.

Public streetscape toolkit will provide a broad framework to be followed while designing a DPR for the streets leading to the heritage zones and plazas mentioned in the CHP document. This toolkit will provide the consultant with a template of broad design and O&M guidelines, which are to be followed while preparation of the design and Detailed Project Report for the streetscape. These guidelines will hence bind the streets and squares of the complete city through street elements including the pedestrian footpath, street furniture, vendor and parking space, signage, security and communication systems, etc. The template is designed by considering the existing condition and the vision of the CHP. A DPR guided by this toolkit will have to prepare by the consultant. For the quick reference, a Reference Catalogue Toolkit is also prepared and attached.

A list of all the elements is presented below followed by a detailed guideline to be followed while designing and selection of vendor for the specific elements listed under the CHP toolkit.

List of elements:

S. No.	Element	S. No.	Element
1	Pedestrian footpath	14	Parking bay
2	Light poles	15	Vendors bay/ platform /Stalls
3	Seating	16	Public conveniences / Toilets
4	Garbage bins	17	Drinking water tap/kiosk
5	Electric poles	18	Cycle stands
6	Bollards	19	Bus stands
7	Information Panels	20	Security cabins
8	Signage	21	Hoarding panels
9	Manhole cover	22	Traffic islands
10	Path for special vehicle	23	Emblems / Posts / <i>Stambhs</i>
11	Service utility bay	24	Compound wall / fencing
12	Storm water Manhole	25	CCTV camera box
13	Cable trench bay		

Though the above list is comprehensive, however for a specific heritage zone certain elements would be designed specifically as per the immediate context. For e.g. certain elements for the temple forecourt zone would have a design specifically for that area. As the immediate context is exclusive, the design of elements may vary a bit to suit to the immediate context.

This in particular would address the need of variety in design and add to the image of the heritage zone.

Guidelines for each element of public streetscape and plaza area.

The following details will highlight the element and macro guidelines to be followed by the DPR consultant for designing the elements. A link to the design guideline, catalogue and annexure are also mentioned with the list which will direct the consultant to design outlines and basic specifications of the element.

S. No.	Parameter	General Guidelines	
A	Pedestrian footpath	Width	<ul style="list-style-type: none"> The pedestrian footpath shall be of minimum 1.5m width* on roads which are more than or equal to 12m RoW. For street width between 4m and 8m the complete street shall be considered as pedestrian footpath. (*Reference IRC, UTIPECC, ITDP guidelines)
		Height	<ul style="list-style-type: none"> The height of pedestrian footpath at any point shall be not more than 150mm from the finished road level.* The slope of the pedestrian footpath shall not be more than 1:100 towards the carriageway or the storm water drain whichever is used for draining of surface water.
		Material	<ul style="list-style-type: none"> Material of pedestrian footpath shall have a good compressive strength, should have good load carrying capacity and shall be repairable and easily washable. It should be non-slippery and shall not reflect heat or reflect harsh sunlight. Moreover the material shall have high resistance to chemicals such as acid, phenyl, or any other chemical which is legally permitted to be used in public areas for washing, cleaning and maintenance purposes. The material shall complement with the locally available material used in the heritage structures and other buildings listed as a part of CHP (refer the Material Catalogue kit). The size of the stone/block/slab shall be such that it can be easily replaced by manual tools and if broken than can be easily replaced. The edge of the stone/block/slab shall be such that it flushes with the walking surface or shall be beveled so that it does not hurt the people walking barefoot on the footpath.
B	<p>Street elements</p> <p>Street elements are the most prominent feature on the street landscape and provide a language and aesthetic beauty in addition to a unique identity to the city. Street elements include everything from a light pole to an inlet of storm water drain. The design, material, and quality of these street elements define the true aesthetic of the town or immediate context. Dwarka being a heritage and pilgrimage town, street elements need to be designed aesthetically complementing the character of the city but at the same time be reliable, sturdy, robust and sustainable. Such a combination is needed so as to decrease long term costs of repair and maintenance and provide simplicity and ease in management to the local body.</p>		
1	Light poles	<p>Lighting of the urban streets is a very important aspect and responsibility of the city administration. Though there are standards defined in the IRC for urban street lighting it is important that for an important tourist destination such as Dwarka, lighting is the most important aspect for improving the urban living. Hence the details for the light poles and light fixtures need to be designed exclusively.</p> <p>Street lights for pedestrian footpath and carriageway are different and carry different specification. Though the light poles on pedestrian footpath shall comply with the UDFI guidelines in its technical specification. However, the aesthetic shall be in sync with the local architecture/language. Use of certain emblems, ornamentation, etc. shall be done while designing the light poles.</p> <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>	
		General guidelines	
		Height of pole	The height of the light pole for the pedestrian footpath from the Finished Floor Level (FFL) of the pedestrian

S. No.	Parameter	General Guidelines				
		<p>footpath shall be between the ranges of 3m to 5m, preferably 4.5m.</p> <p>General guidelines</p> <table border="1"> <tr> <td>Fixture</td> <td> <ul style="list-style-type: none"> - The fixture shall be white LED and having a high lumen quality and a Class-1 quality Ingress Protection (IP). The fixture shall be of an international grade quality and comply with IEC standard. - Shall have good service-maintenance provision available in the region. </td> </tr> <tr> <td>Junction Box</td> <td>The JB shall be encased within the shaft/ base of the light pole.</td> </tr> </table>	Fixture	<ul style="list-style-type: none"> - The fixture shall be white LED and having a high lumen quality and a Class-1 quality Ingress Protection (IP). The fixture shall be of an international grade quality and comply with IEC standard. - Shall have good service-maintenance provision available in the region. 	Junction Box	The JB shall be encased within the shaft/ base of the light pole.
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Junction Box	The JB shall be encased within the shaft/ base of the light pole.					
2	Seating	<ul style="list-style-type: none"> - The seating shall be made of robust and strong material preferably Stone or cast iron. Though any design shall be provided which can be built in mass production and should be replaceable if broken. - It shall be designed that it doesn't accumulate any rain water in the seating space. - The material of the seating space shall remain temperate during any given season be it heat or cold and shall be resistant to electric shocks. - The bench or seating should be robust enough to avoid theft <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>				
3	Garbage bins	<ul style="list-style-type: none"> - The garbage bins shall be made of robust and strong material preferably reinforced FRP. Though any design which is provided should be capable of mass production and should be replaceable if broken. The mouth of the garbage bin should not be such that any stray animal can access it easily in order to avoid filth. - The municipal SWM workers should be able to comfortably empty these bins for maintenance purpose. - The height of the bins shall be such that it is comfortable for the users especially for children and women. - If possible the design should incorporate a wet and dry compartment within the design. Moreover it should not accumulate water and the inner surface of the bin shall be such that it can be cleaned easily without use of domestic chemicals. - The bin should have the facility to bolt it to the floor or ground as a preventive measure of theft and vandalism. <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>				
4	Electric poles	<p>As the electricity department is a state subject, and Paschim Gujarat Vij Company Ltd. (PGVCL) is the service provider, the electricity poles and electric cables if needed to be rearranged, then the designs and permissions will be needed from the PGVCL.</p> <p>General guidelines</p> <table border="1"> <tr> <td>General guidelines for Electric pole</td> <td> <ul style="list-style-type: none"> - The design of the pole shall be for the post of the electric pole only. - The material and design shall be in complementary to the street furniture and other elements of the build façade. - The height shall be in concurrence to the standards provided by the electricity company. </td> </tr> </table>	General guidelines for Electric pole	<ul style="list-style-type: none"> - The design of the pole shall be for the post of the electric pole only. - The material and design shall be in complementary to the street furniture and other elements of the build façade. - The height shall be in concurrence to the standards provided by the electricity company. 		
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5	Bollards	<p>Bollards are important elements as it acts as physical barrier and also enforces discipline in users. Bollards are typically used in urban areas for restricting entry, provide barrier, and enforces traffic discipline amongst citizens. Bollards are sometimes also used to restrict entry of stray animals.</p> <p>General guidelines</p> <table border="1"> <tr> <td>General guidelines for bollards</td> <td> <ul style="list-style-type: none"> - Bollards used in the streets shall be used to define and restrict entry point across a defined limit. - The design of the bollard shall be as per design specifications. Though at places where necessary bollards can be designed as per the site condition and traffic flow. - The spacing between bollards shall be as per the IRC standards </td> </tr> </table>	General guidelines for bollards	<ul style="list-style-type: none"> - Bollards used in the streets shall be used to define and restrict entry point across a defined limit. - The design of the bollard shall be as per design specifications. Though at places where necessary bollards can be designed as per the site condition and traffic flow. - The spacing between bollards shall be as per the IRC standards 		
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S. No.	Parameter	General Guidelines	
			- The bollards shall be robust and fixed/attached to the ground/Floor
6	Information panel	The information panel shall consist of information regarding the heritage zone or selected area nearest heritage structure of Dwarka, places of interest around Dwarka, medical and emergency facilities in Dwarka, timings and other details about the main temple and upcoming and annual time table of festivals and important occasions for tourists.	
		General guidelines	
		Space / location	- The spatial distribution of the information panel across the heritage zones shall be as per the detailed spatial plan of that particular heritage zone.
		Design specification	A design guideline and reference catalogue is provided in the next section.
		O & M specifications	<ul style="list-style-type: none"> - The information panel can be an electronic panel, digital panel or a static panel on the discretion of the designer and type of information to be deciphered - It shall be such that it can be easily mounted on a designated space whether floor or wall or pole mounted and should be possible to be fixed with a regular mild steel frame - The panel if is digital in nature than shall be designed so as to protect the inspection chamber digital panel and visual screen from rainwater. - Moreover it should be water proof for easy O&M and sustainable usage.
7	Signage	The signage shall display information regarding way finding, directions, speed limits, etc. Moreover there shall be a hierarchy of signage for city level, Heritage zone area, way-finding, public amenities and conveniences, etc. The signage for the city area and heritage zones shall be different so as to enhance and highlight the heritage zone. Though the signage for main temple shall be different from all other areas.	
		General Guidelines	
		Space / location	The spatial distribution of the signage shall be as per the detailed spatial plan of particular heritage zone provided as a part of Heritage Zone Plans by the HCA.
		Design specification	A design guideline and reference catalogue is provided in the next section.
		O & M requirements	<ul style="list-style-type: none"> - The signage shall be easily mountable at the designated area shown in spatial plan. - The material of the signage board as well as its pole shall be easily cleanable and shall be weather proof (as per local context) - The paint/stickers shall be as per the IRC standards mentioned in IRC.067.2012 - It shall be made such that it is easily replaceable if damaged and shall be easily stored and available. - It shall have a provision of concealed electrical conduit, wherever CCTV cameras or digital signage are to be mounted.
	Language	The language of the fonts in the signage shall be in Hindi, Gujarati and English. Although the fonts/font size used shall be as per the standards and codes mentioned in the IRC.067.2012.	
8	Path for special vehicle	The special vehicle path is a path along the pedestrian footpath allocated specially for vehicles such as bicycles, wheelchairs, battery operated carts, etc. used by senior citizens, physically challenged, children, etc.	
		General guidelines	
	Width	The width shall be between 1.5 m and 2 m so as to easily accommodate a transit vehicle such as battery operated car/ wheelchair/ etc.	

S. No.	Parameter	General Guidelines	
		Height	This path shall be flushed with the adjacent pedestrian footpath and shall be 150mm lower than the property line.
		Material	The path should be built of compacted earth or PCC bed with broom finish surface of pigmented or self-colour paint. It should be capable to be cleaned, swept and maintained easily.
9	Service utility bay	The service utility bay shall be designed incorporating all the existing underground services in the city in addition to any future additions such as storm water line, fire-fighting line, etc. The service utility bay shall have enough depth and width underground so as to accommodate the services.	
	Drainage/ Storm Water Manhole	<p>In case it is on pedestrian footpath: The manhole shall be lined with a cast-in-situ RCC slab so as to prevent damage to the manhole in case the stone or block is damaged. The manhole cover shall be made of stone similar to material of pedestrian footpath and shall be heavy enough so as to prevent theft or vandalism.</p> <p>In case it is on carriageway: The manhole shall be as per the standard procedure carried by the municipality.</p>	
	Cable utility trench	<p>Cable utility trench should be provided along with the service utility bay which shall carry all the underground conduits such as communications and fibre-optic conduits, electricity. This trench shall be made of precast rcc and shall be placed adjacent to the service utility bay towards the property side. This trench shall be covered with stone, precast or cast iron open able cover which can be removed whenever any servicing is needed. The width and height of this trench shall be in concurrence to the standards decided by the electricity service provider.</p> <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>	
10	On street Parking bay	<ul style="list-style-type: none"> - The parking bay can be accommodated along the pedestrian footpath - In case of plaza 15% of the plaza space shall be used for parking of IPT and public transport vehicles and 5% for private vehicles. - The parking areas shall be lined with cat-eye reflectors and clear signage with capacity and charges (if applicable) mentioned shall be strategically placed. - In case of plaza, the parking bay shall be delineated by lining it with bollards, kerbs, paint marking or reflectors <p style="text-align: center;">‘OR’</p> <ul style="list-style-type: none"> - Using a different material, than the material used to surface the plaza. 	
11	Vendor’s bay	<p>Vendors are an integral part of the tourist destination. Dwarka has vendors involved in varied types of activities from food-stalls to selling merchandise for pilgrims. Moreover the vendor spaces within the plaza exhibits different activity pattern throughout the day.</p> <ul style="list-style-type: none"> - A spatial plan for vendor’s bay for every selected heritage zone is being prepared under CHP report. - The designated space shall be clearly defined with a different use of surface material. - Every vendor bay shall have enough lighting and surveillance provision through installation of CCTV cameras. - It shall have a clearly defined and designated space for seating area. - The vendor bay shall have enough numbers of garbage bins placed alongside. - A number of places, the Vendors shall be provided a permanent Kiosk, which is to be designed suiting the local context and language. 	
12	Public conveniences / Toilets	<p>Public conveniences are strategic designs to ensure cleanliness and hygiene of the space. In an area of heritage importance it is necessary that the design of such elements/buildings match to the context and are convenient and comfortable enough for the users of all age group. Moreover proper availability of water, use of strong materials and fixtures and a hygienic environment are necessary.</p> <ul style="list-style-type: none"> - The public convenience shall have separate male/female toilets for use. - It shall be accessible friendly and shall follow revised guidelines and space standards for barrier free built environment issued by CPWD. - It shall have enough lighting and flushing facilities with hand cleaning facilities for users. - The doors shall not open towards the street directly 	

S. No.	Parameter	General Guidelines
		<ul style="list-style-type: none"> - Moreover, provision of proper ventilation systems and connection to main drainage line shall be designed. - There should be a designated bay for housing the inspection chambers. - The height of the toilet plinth shall be such that rain water flooding or animals do not enter the toilet premises and make it dirty. - The water tank above the toilet block if not a built structure, shall be screened with a 'stone jaali'. - Allocation of a separate cabinet for storage of equipment for O&M shall be provided in the toilet layout. - There should be enough buffer alongside the ventilation façade for landscape or tree plantation to avoid foul smell spillage over immediate activities. - The other facades should have designated areas for pasting banners and cleanliness awareness posters. - The toilet block shall have provision of CCTV cameras housed externally, facing towards carriageway for security and surveillance. <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>
13	Public conveniences/ Drinking water station and kiosks	<p>Public conveniences are strategic designs to ensure cleanliness and hygiene of the space. In an area of heritage importance it is necessary that the design of such elements/buildings match to the context and are convenient and comfortable enough for the users of all age group. Moreover, use of strong materials and fixtures and a hygienic environment are necessary.</p> <ul style="list-style-type: none"> - It shall be accessible friendly and shall follow revised accessibility guidelines published by central government. - It shall have a proper water drain system so that the minimum spillage of water occurs and the access areas remain clean and dry. - It shall have enough lighting and unused water disposal facility - The water kiosks shall use durable and strong fixtures and materials which are easily repairable and available. - The access area and base plinth of the water kiosk shall be such that water accumulation does not occur and the material used for the floor is non slippery. - In case a water filter system is used, it shall not be in open and shall be protected well with a locked area - Water supply connection if not available then a proper overhead storage shall be provided. <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>
14	Cycle stands	<p>As the city is a tourist city, the CHP focuses on sustainability and hence encourage the use of bicycle within the heritage area of Dwarka. Cycle parking facilities are hence important and shall be such that the cycles are protected against theft.</p> <ul style="list-style-type: none"> - The cycle stand shall not be of metal due to climatic conditions - It shall be designed so that there is a provision of access from sideways for a cyclists. - It may be with or without shelter and be fixed to the floor or ground. - Shall be monitored through CCTV camera.
15	Bus Stops	<p>Though local transport buses do not enter the city heritage area, however private buses enter the heritage city of Dwarka. Bus stops are important urban elements which contribute to the image of the city. Bus stops also tend to be element which can disseminate information and can also generate revenue through advertisement or banner display.</p> <ul style="list-style-type: none"> - The bus stop shall be designed such that it can be easily identified by commuters as well as drivers. - The bus stop shall have a dedicated bus bay for parking of the bus. This shall be designed in detail by DPR consultant in the allocated space provided in CHP - The bus stop shall be designed by considering allocated space for information panel, CCTV, advertisement space, enough lighting, and space for digital signage.

S. No.	Parameter	General Guidelines
		<ul style="list-style-type: none"> - The reference design guideline provided in the toolkit can be changed as per site context and space availability. Though the basic guidelines shall be followed. <p><i>A design guideline and reference catalogue is provided in the next section.</i></p>
16	Security cabins	<p>At all locations marked in the CHP, it is recommended that Security cabins shall be designed for surveillance and security purposes. The consultant shall in discussion with local Security, design this cabins as per the requirement and approve it through the HCA.</p> <ul style="list-style-type: none"> - It is recommended that the consultant should follow the local architectural aesthetics for such cabins. - All other details can be on the discretion of the consultant.
17	Shop panels/ signage	<ul style="list-style-type: none"> - The propriety panels shall be in the ratio of 1:2 (height: width). The DPR consultant shall provide basic framework of the use of material, colour palette and fonts to be used for written work. - The DPR consultant shall, based on the design guidelines prepare a byelaw to be included in the GDCR in consultation with the HCA.
18	Public art or Sculpture	<p>At various designated locations within the heritage zones, there will be allocated space for local artisans, sculptors, painters or creators for showcasing art of the local context or heritage and culture.</p> <ul style="list-style-type: none"> - The DPR consultant shall design a space at the allocated locations in the heritage zones for showcase of such works. - The space shall be accessible and shall have provision of adequately lighting the space. - The space shall have hard flooring and be clean and hygienic inaccessible by vehicles and animals.

Street elevation guidelines for conservation of heritage in Dwarka

RESPECTING THE HERITAGE BUILDING ELEVATIONS



HERITAGE BUILDING: INFORMATION CENTRE

Maintaining the semi circular arched openings in the elevations of the buildings



COLONIAL STRUCTURE: DHARAMSHALA (GUEST HOUSE)

Maintaining the traditional building parapets

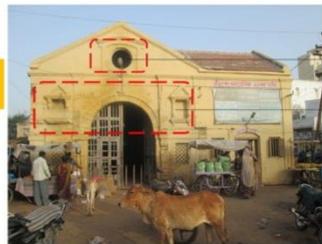
Maintaining the colonial semicircular arches on the building elevations

Maintaining the brackets in the building facade



STREET ELEVATION VEGETABLE MARKET SQUARE

Individual building heights to be maintained



HERITAGE STRUCTURE: VEGETABLE MARKET

Old windows in on building facade to be maintained

Plaster work to be conserved



Building heights to be equal to the heritage structures

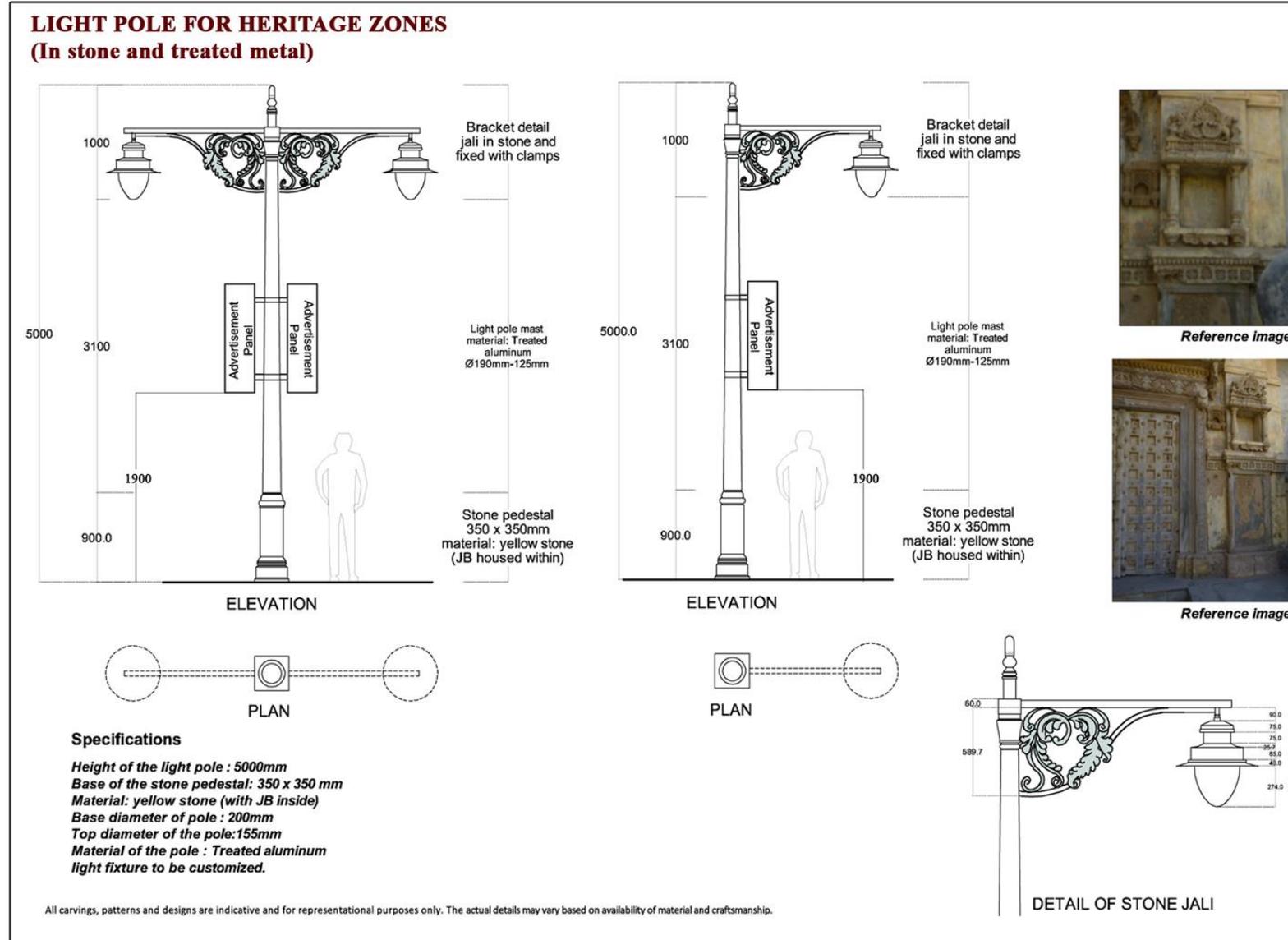
Street scape issues and considerations during preparation of city conservation plan

1. On the road side the building line should aligned.
2. Wires and cables should concealed on the front façade.
3. Building Projection should not more than 1.2m
4. The height of the all shop's Hoardings should at the 2.7 m from finished footpath
5. Temporary shade should not projected more than 1m.
6. Height of the signage/ Hoardings should no more than 0.75m and width should not exceed than a front facade.

Building Guideline Toolkit and Pallet Creation

The building guidelines (bye-laws) prepared by the Archaeological Survey of India for the prohibited and regulated zones around the Dwarkadhish Temple apply to all Heritage Zones proposed in this CHP. The guidelines and pallet has been provided below.

Light poles



Street benches

SEATING BENCHES FOR THE FIVE HERITAGE ZONES IN DWARKA

FRONT ELEVATION OF THE SEAT
 Dimensions: Total length 1500.0, seat length 990.1, seat width 80.0, distance from pillar to seat edge 161.0 and 161.1.

TOP VIEW - STREET BENCHES
 Dimensions: Total length 1497.3, seat length 990, seat width 450.0, distance from pillar to seat edge 93.8 and 160, seat width 95.

BENCH WITH BACKREST SEATING
 Dimensions: Total length 699.0, seat length 300.0, seat width 70.0, pillar height 329.0.

SIDE ELEVATION
 Dimensions: Total height 440.0, seat width 360.0.

SIDE ELEVATION
 Dimensions: Total height 145 (47+88+25+52+25+15+20+5+30+15), seat width 360.0.

Specifications:
 length of the seating: 1500mm
 Breadth of the seating: 450mm
 Height of the seating : 440mm
 Material: yellow sand stone
 (preferable local material)

Reference Heritage Components
 Ornamentation and motifs reference from the local architecture seen on the temple cornices, walls and jharokas.

The seating benches are to be installed in all the heritage zones for resting of the visitors. The benches are made in local stone that are durable and in context with the heritage of Dwarka.

All carvings, patterns and designs are indicative and for representational purposes only. The actual details may vary based on availability of material and craftsmanship.

Bollards

BOLLARDS FOR TRAFFIC CONTROL

ELEVATION
 200.0
 850

PLAN
 200.0

DETAIL OF CARVING IN STONES
 10.0
 55.0
 10.0
 5.5 40

SECTION AA'
 30.0
 14.5
 18.5

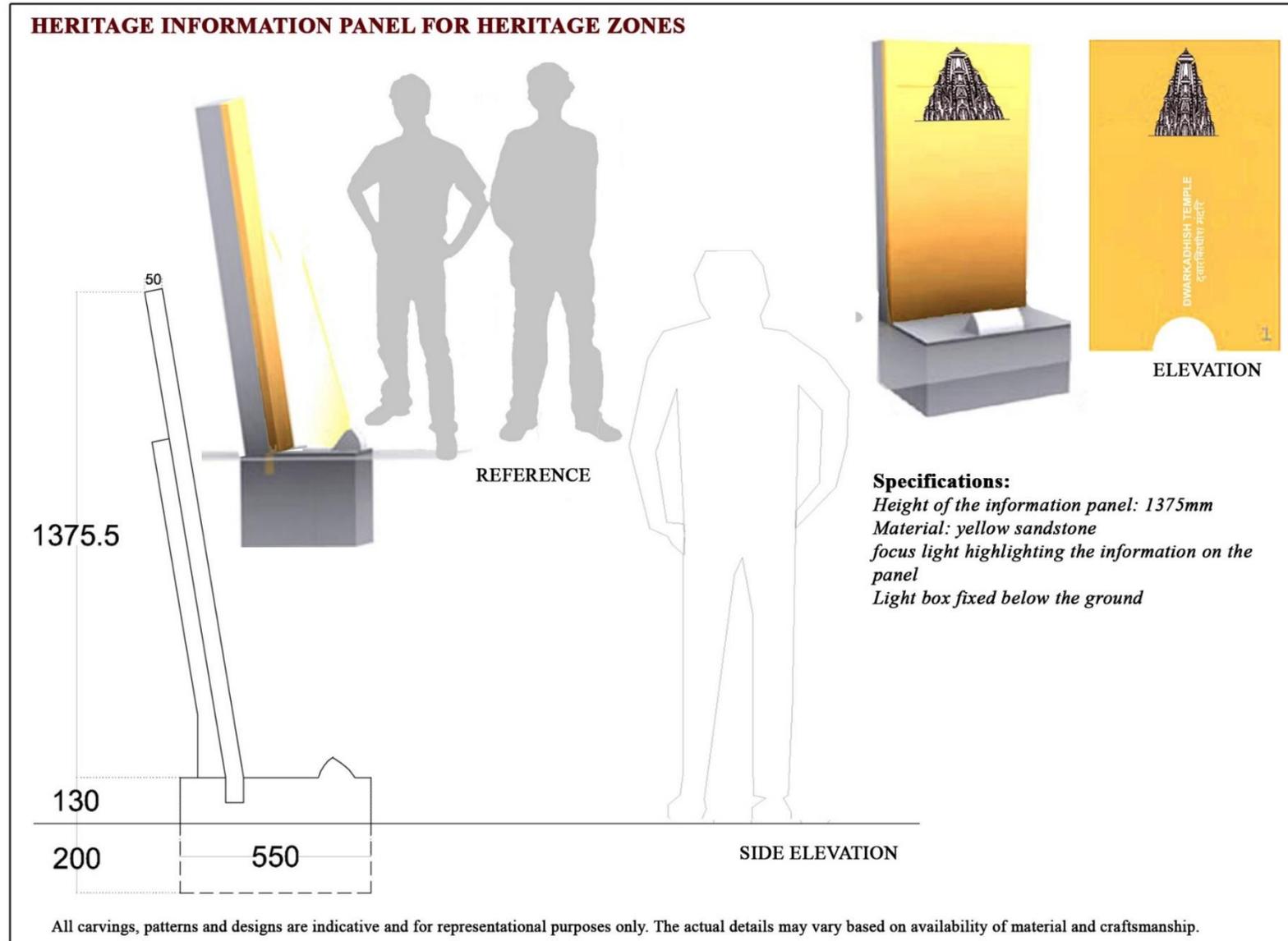
Specifications:
 Diameter of bollards: 200mm
 Height of the bollard : 730mm
 Material: yellow stone
 (preferable local material)

Reference Heritage
 Ornamentation and motifs
 reference from the local ar-
 chitecture seen on the temple
 cornices, walls and jharokas.

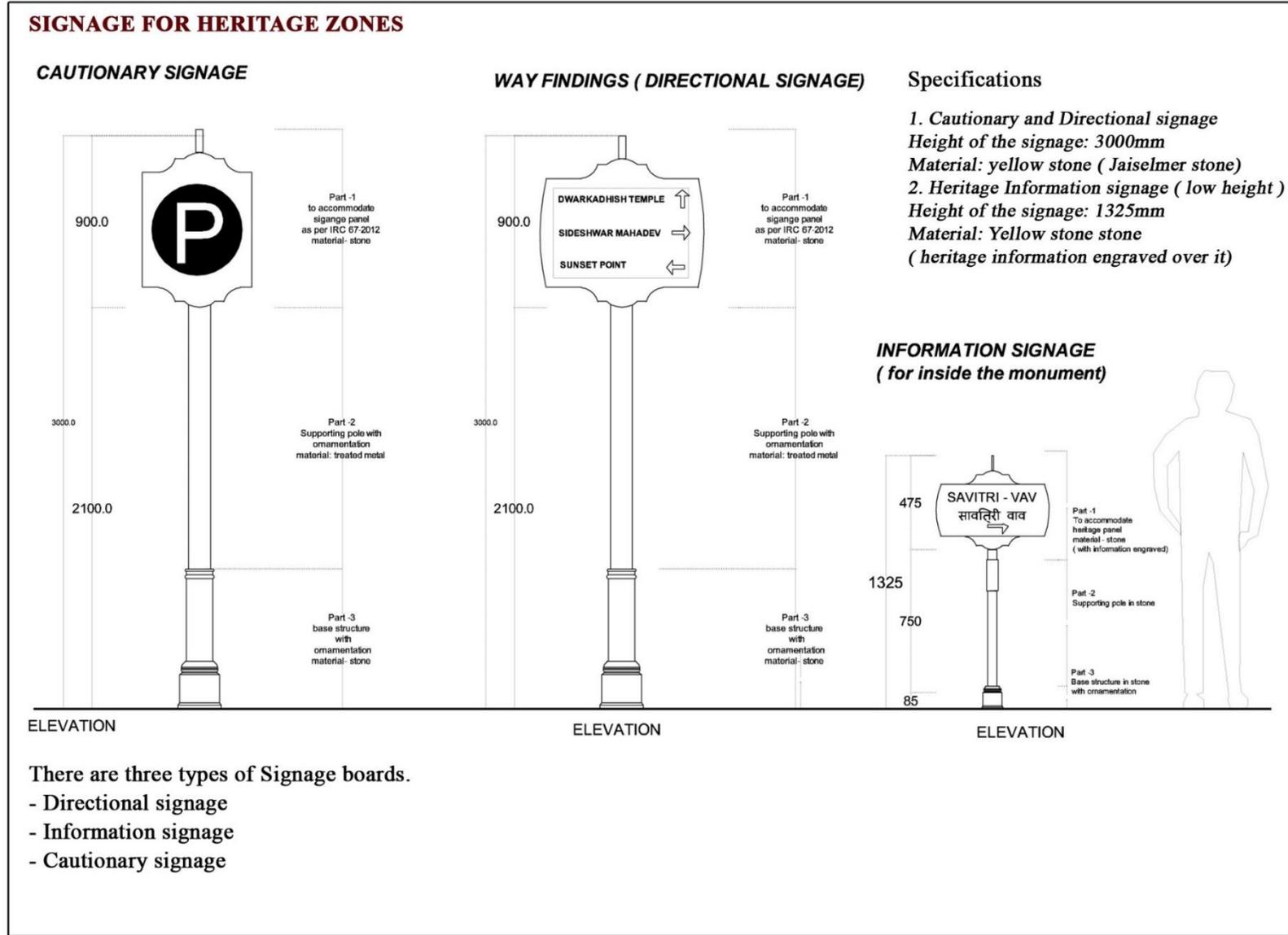
*The bollards are design so as to control the movement of traffic
 in the public areas in the five priority heritage zones.*

All carvings, patterns and designs are indicative and for representational purposes only. The actual details may vary based on availability of material and craftsmanship.

Information Panel



Signage



Public conveniences (Toilet block)

TOILET BLOCK FOR HERITAGE ZONES

FRONT SIDE ELEVATION A

FRONT SIDE ELEVATION B

ELEVATION

PLAN

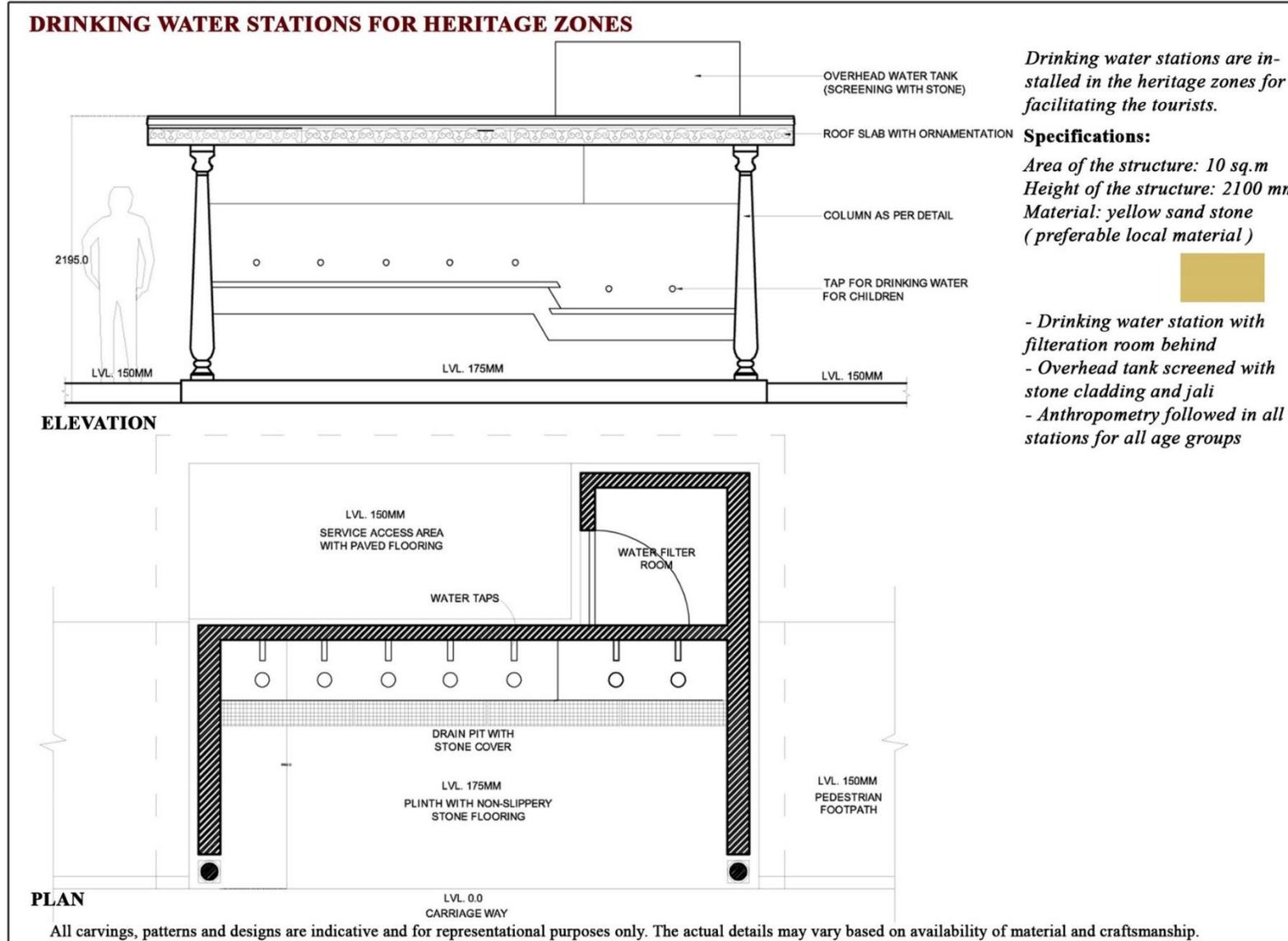
The level of reference between the road level and the path of the toilet to be considered as per the site and 1:10 slope for physically challenged people.

Specifications:

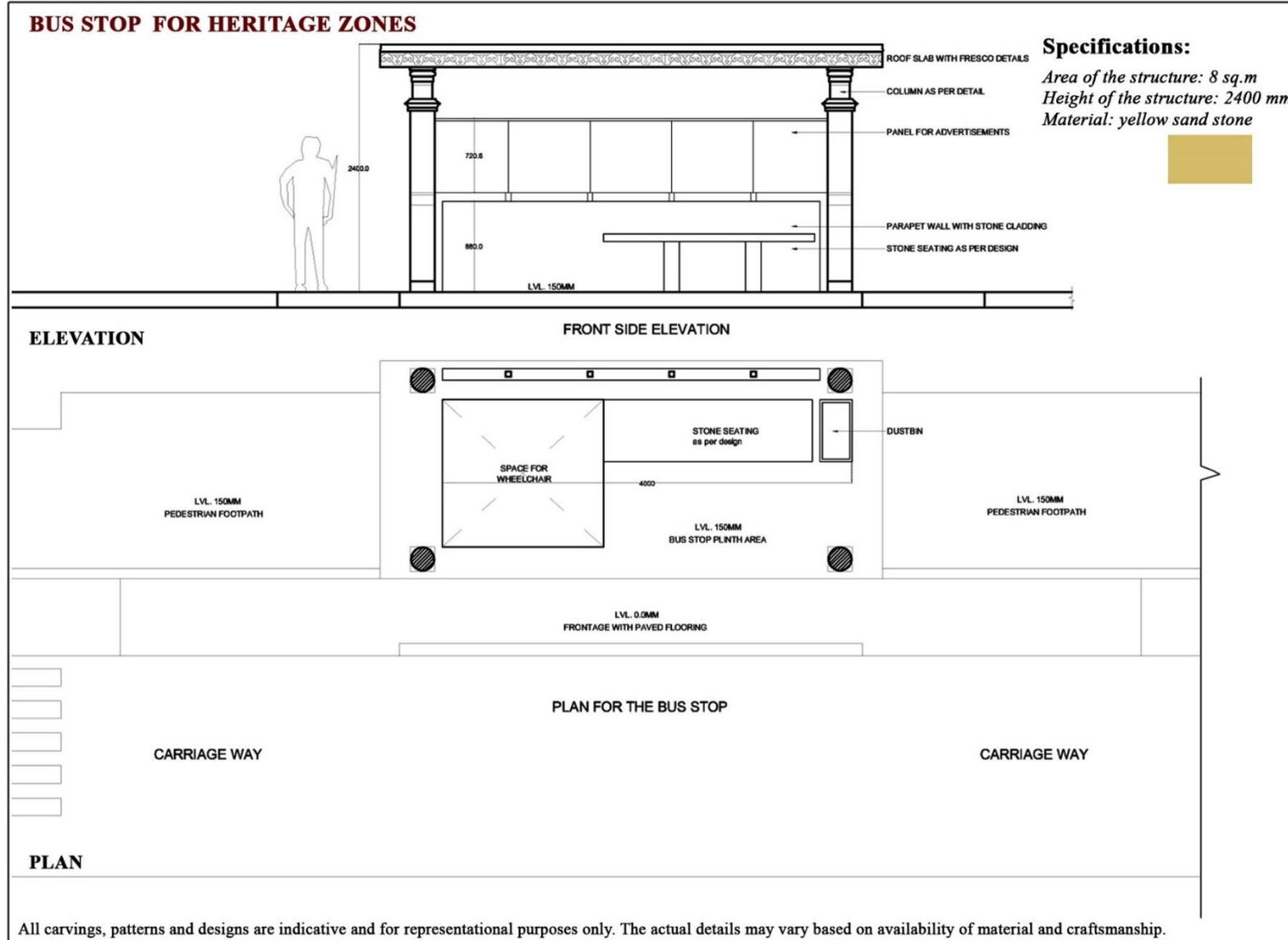
- Accessibility for physically challenged considered*
- length of the toilet block: 4000 mm*
- Breadth of the toilet block : 1850 mm*
- Height of the toilet block: 7 sq.m*
- Area of the structure: 2100 mm*
- No. of seats in each: 2*
- Material: yellow sand stone and metal (preferable local sandstone)*

All carvings, patterns and designs are indicative and for representational purposes only. The actual details may vary based on availability of material and craftsmanship.

Public conveniences (Drinking water block)



Bus-stop



Security cabin

SECURITY CABIN FOR FORECOURT OF DWARKADHISH TEMPLE AND VEGETABLE MARKET SQUARE (Heritage zone 1 and 3)

Specifications:
 Area of the structure: 5.8 sq.m
 Height of the structure: 2200mm
 Material: yellow sand stone
 (preferable local material)

Window frame : glass fixed in metal frame

Reference Heritage Components
 Columns and walls built taking refernce from the local architecture seen on the temple and other heritage structure like Old Ramvadi. Ornamentation similar as seen on the heriage structure.

The security cabin is an important chekxpost needed at the entrance of the Dwarkadhish temple for the security and information, the structure proposed is built in context to the temple in the local materials and similar achitecture .

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Bibliography